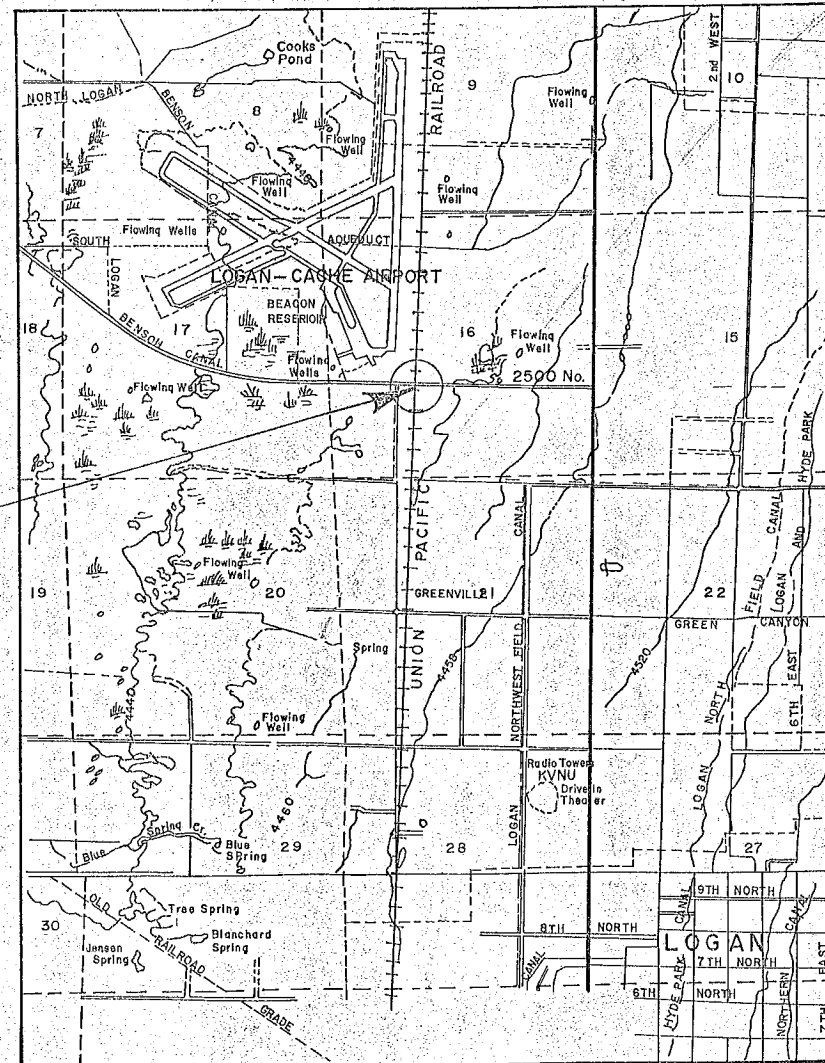


UTAH DEPARTMENT OF TRANSPORTATION

PLANS OF PROPOSED STATE ROAD FEDERAL AID PROJECT RRS-0005(6) RAILROAD CROSSING IMPROVEMENT 2500 NORTH 400 WEST LOGAN, CACHE COUNTY LENGTH 0.0013 MILE GRADING & SURFACING

R.R.S. 0005(6)
2500 NORTH 400 WEST
LOGAN
R.R.M.P. 71.06



FOR PRELIMINARY REVIEW ONLY
SUBJECT TO REVISION

FED. ROAD DIST. NO.	UTAH	PROJ. NO.	SHEET NO.
UTAH	UTAH	RRS - 0005(6)	

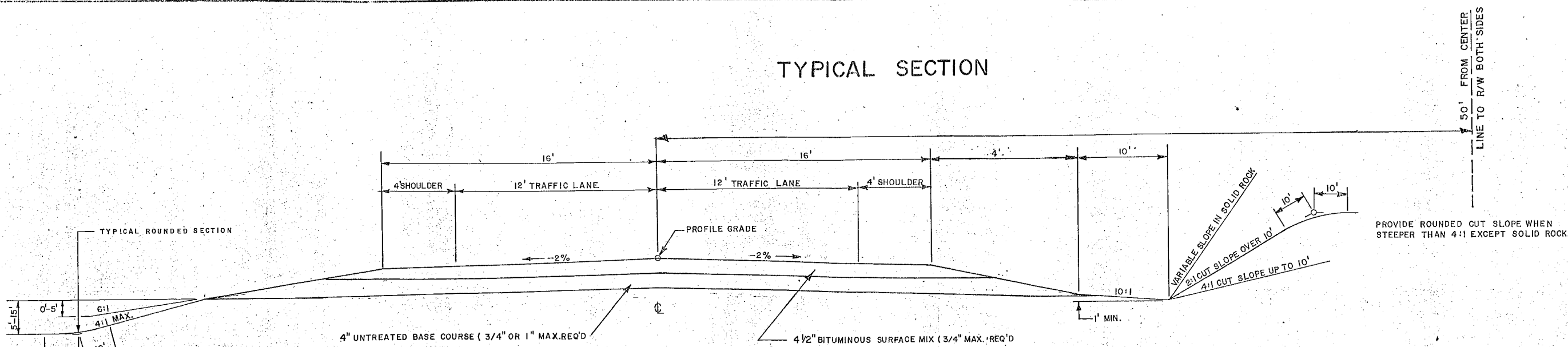
ROADWAY DRAWINGS	
SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	TYPICAL SECTIONS
3	SUMMARY SHEET
4	TRAFFIC CONTROL SHEET
4-B	" " "
4-G	" " "
4H, 4K	" " "
5	PLAN AND PROFILE SHEET

NO.	DATE	STANDARD DRAWINGS
745-IA	5-4-82	CONST. SIGNING CHANNELIZATION DEVICES
745-IB	5-2-82	CONSTRUCTION NOTIFICATION SIGNS
745-IC	7-8-80	CONSTRUCTION SIGNING BARRICADES
745-47	5-9-78	TRAFFIC CONTROLS AT R.R. CROSSING
815-3	12-17-81	STANDARDS FOR RURAL TWOLANE HWYS

UTAH DEPARTMENT OF TRANSPORTATION
 RECOMMENDED FOR APPROVAL _____ 19
 CHIEF, ROADWAY DESIGN
 RECOMMENDED FOR APPROVAL _____ 19
 ENGINEER FOR PRECONSTRUCTION
 APPROVED _____ 19
 ASSISTANT DIRECTOR

US DEPARTMENT OF TRANSPORTATION
 FEDERAL HIGHWAY ADMINISTRATION
 APPROVED _____
 DIVISION ADMINISTRATOR

TYPICAL SECTION



NOTE: ANY BORROW EXISTING OR PLACED BENEATH THE UNTREATED BASE COURSE SHALL BE GRANULAR (I.E., AASHTO SOILS CLASSIFICATION A-1) TO CURTAIL CAPILLARY RAISE OF WATER.

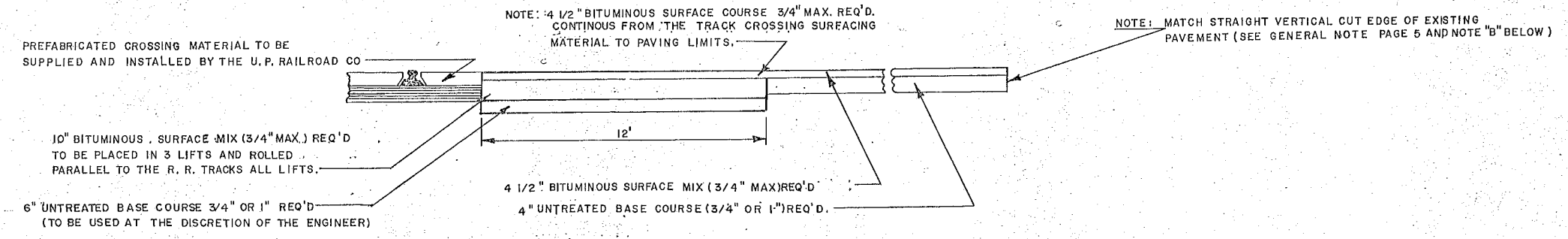
NOTE: THE BITUMINOUS SURFACE MIX SHALL UTILIZE AN AC-10 VISCOSITY GRADED ASPHALT. THE BITUMINOUS SURFACE MIX SHALL BE PLACED AND COMPACTED IN TWO APPROXIMATELY EQUAL COURSES OR LIFTS.

SECTION "A"
 STA. 0+65.5 TO STA. 1+34.5
 DESIGN SPEED 55 MPH

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RAILROAD CROSSING APPROACH DETAIL

NOTE: TO BE USED ON BOTH APPROACHES.



TYPICAL SECTION NOTE

- CROWN MUST BE WARPED OUT TO MATCH TRACK ELEVATION. USE 25 FOOT TRANSITION.
- WHERE THE EXISTING PAVEMENT IS CUT AT PAVING LIMITS IT SHALL BE CUT VERTICALLY IN A STRAIGHT LINE AND BE FREE FROM BROKEN EDGES.
- ALL EXISTING BITUMINOUS SURFACE WITHIN THE PROJECT LIMITS SHALL BE REMOVED AS ROADWAY EXCAVATION.
- THE 10" THICK LAYER OF BITUMINOUS SURFACE MIX SHOWN IN THE APPROACH DETAIL (APPROACH SLAB) MUST BE PLACED IN A SINGLE CONTINUOUS OPERATION.

UTAH DEPARTMENT OF TRANSPORTATION			
SALT LAKE CITY, UTAH			
ROADWAY DESIGN			
RAILROAD CROSSING IMPROVEMENT CACHE COUNTY			
2500 NORTH 400 WEST			
TYPICAL SECTION SHEET			
DESIGN K.M.V.	10/5/82	CHECK	REVIEW
DRAWN K.M.V.	10/19/82	CHECK	DESIGN
QUANT.		CHECK	P.R.V.
APPROVAL	[Signature]		PROJECT ENGINEER
RECOMM.	[Signature]		PLANS & ESTIMATES ENGINEER
APPROVED	5-10-83	[Signature]	CACHE COUNTY
PROJECT NUMBER	R.R.S.-0005(6)		CHEAT NO. 2

NO.	BY	DATE	REMARKS

EARTHWORK & SURFACING

LINE	STATION	LENGTH	ROADWAY EXCAVATION				BORROW				UNTREATED BASE COURSE 3/4" OR 1" MAX 135 LB/CU FT			BITUMINOUS SURFACE MIX 3/4" MAX 149 LB/CU FT			BITUMINOUS MATERIALS*					CONSTRUCTION SOURCE REFERENCE
			FEET	AVE AREA SQ FT	CU YD	AVE AREA SQ FT	CU YD	AVE WIDTH FEET	DEPTH INCHES	TON	AVE WIDTH FEET	DEPTH INCHES	TON	AVE WIDTH FEET	TON	AVE WIDTH FEET	TON	TON	LB			
																				PRIME COAT BITUMINOUS MATERIAL GRADE MC-70 OR MC-250 0.30 GAL/SQ YD 249 GAL/TON	BITUMINOUS TACK COAT 0.08 GAL/SQ YD 249 GAL/TON	
1	0+65 - 0+83.5	18	19	13	32	21	37.5	4	15	33.9	4.5	17	33.9	0.08	32.9	0.02	1.02	6				
2	APPROACH SLABS 0+83.5 - 85.5 & 1+04.5 - 16.5	24	21	19	7.5	7				38	14.5	82	38	0.12	35.0	0.12	4.92	25				
3	1+16.5 - 1+34.5	18	14	9	36.5	24	37.5	4	15	33.9	4.5	17	33.9	0.08	32.9	0.02	1.02	6				
TOTAL				41		52			30			116		0.3		0.2	7.0	37				
USE				41		52			30			116		*		*	*	*				

*FOR INFORMATION ONLY, NOT A BID ITEM

ITEM	UNIT	QUANTITY	CONSTRUCTION SOURCE REFERENCE
MOBILIZATION	LUMP	0	
FLAGGING	HOUR	100	
RAILROAD CROSSING PAVEMENT MARKING (WHITE 0.4 + YELLOW 0.2 + 0.6 - USE 1)	EACH	2	
HIGHWAY TRAFFIC PAINT	GAL	1	
CONTINGENT SUM PAY ITEM	LUMP	0	

ITEM	UNIT	QUANTITY
MOBILIZATION	LUMP	0
FLAGGING	HOUR	100
ROADWAY EXCAVATION	CU YD.	41
BORROW	CU YD	52
UNTREATED BASE COURSE 3/4" OR 1" MAX	TON	30
BITUMINOUS SURFACE MIX 3/4" MAX	TON	116
RAILROAD CROSSING PAVEMENT MARKING	EACH	2
HIGHWAY TRAFFIC PAINT	GAL	1
CONTINGENT SUM PAY ITEM	LUMP	0

FOR PRELIMINARY REVIEW ONLY
SUBJECT TO REVISION

UTAH DEPARTMENT OF TRANSPORTATION
SALT LAKE CITY, UTAH
ROADWAY DESIGN

RAILROAD CROSSING IMPROVEMENT
2500 NORTH 400 WEST
SUMMARY SHEET
RRS-0005(6)

DESIGN WAG 08/84
DRAWN RGO 09/84
DATE 2-14-84
PROJECT DESIGN ENGINEER
APPROVED 2-14-84
DATE 2-14-84
ESTIMATES ENGINEER

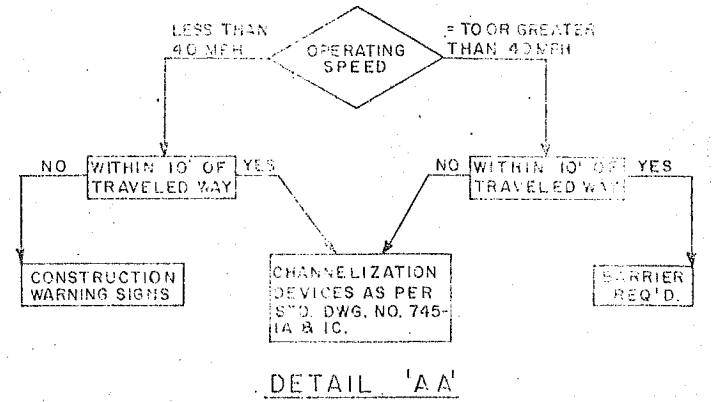
REVISIONS

NO. DATE DESCRIPTION REVISIONS BY

1 ORIGINAL SUBMISSION FOR AUTHORIZATION

TRAFFIC PHASING

NOTES:



- 1. 'ROAD WORK AHEAD' SIGNS SHALL BE ACCEPTABLE ALTERNATES TO 'ROAD CONSTRUCTION AHEAD' SIGNS. 'END ROAD WORK' SIGNS SHALL BE ACCEPTABLE ALTERNATES TO 'END CONSTRUCTION' SIGNS.
- 2. ALL SIGNS, PROCEDURES, AND METHODS USED IN THE CONTROL OF TRAFFIC SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," EDITION OF 1978 SECTION VI AND WITH UTAH STATE STANDARD DRAWINGS 745-1A, 1B, 1C
- 3. ADVANCE WARNING DEVICE CODE TO BE SPECIFIED BY PROJECT ENGINEER.
- 4. SIGNS USED ON ROADS WITH POSTED SPEED LIMITS LESS THAN 40 M.P.H. SHALL HAVE A 5' MINIMUM MOUNTING HEIGHT.
- 5. ALL EMPLOYEES VEHICLES, STOCKPILED MATERIALS, AND EQUIPMENT SHALL BE REMOVED FROM THE ROADWAY CLEAR ZONE. IF REMOVAL IS IMPRACTICAL SEE DETAIL 'AA' THIS SHEET FOR PROTECTION.
- 6. CONES SHALL NOT BE ALLOWED FOR USE AS CHANNELIZING DEVICES FOR NIGHT TIME OPERATIONS.
- 7. ALL SIGNS AND DEVICES SHALL BE INSPECTED AT INTERVALS DETERMINED BY THE PROJECT ENGINEER. ANY SIGNS OR DEVICES DETERMINED TO BE IN NEED OF MAINTENANCE, REPLACEMENT OR REPOSITIONING SHALL RECEIVE SUCH ATTENTION BY THE CONTRACTOR AS SOON AS POSSIBLE. SAID MAINTENANCE SHALL INCLUDE CLEANING OF DEVICES AT LEAST ONCE A WEEK.
- 8. THE FLAGGER SYMBOL SIGN SHOULD BE SUPPLEMENTED BY A DISTANCE PLATE WHEN FLAGGING STATION IS NOT VISIBLE FROM SIGN.
- 9. STANDARD PAVEMENT MARKINGS OR MUTCD CHANNELIZATION DEVICES OTHER THAN CONES SHALL BE PROVIDED DURING NON-WORKING HOURS.
- 10. TRAFFIC CONTROL PLAN ITEMS NOT LISTED AS SPECIFIC BID ITEMS SHALL BE PAID FOR IN THE ITEM "MOBILIZATION"
- 11. DURING NON-WORKING HOURS ALL NON-APPLICABLE SIGNS AND/OR DEVICES SHALL BE REMOVED 20' FROM THE ROADWAY OR BEHIND CURB AND GUTTER. THE SIGN FACE SHALL BE TURNED 90° FROM THE ROAD. NON-APPLICABLE POST MOUNTED SIGNS SHALL BE COVERED.
- 12. THE CONTRACTOR MAY SUBMIT AN ALTERNATE TRAFFIC CONTROL PLAN. THIS PLAN SHALL BE SUBMITTED NO LATER THAN THE PRE-CONSTRUCTION CONFERENCE. ANY DEVIATION FROM THIS PLAN MUST BE APPROVED BY THE ENGINEER IN WRITING.
- 13. ANY CONSTRUCTION HAZARD OR SLOPE STEEPER THAN 3:1 AND BETWEEN 6" AND 48" IN DEPTH WITHIN 20' OF THE TRAVELED WAY SHALL BE PROTECTED IN ACCORDANCE WITH DETAIL 'AA', THIS SHEET. SLOPE DEPTHS GREATER THAN 48" SHALL BE PROTECTED BY DEVICES AS RECOMMENDED BY THE MDT DIVISION OF SAFETY.
- 14. NON-APPLICABLE TRAFFIC MARKINGS SHALL BE PHYSICALLY REMOVED (NOT PAINTED OVER) AND APPROPRIATE MARKINGS PUT IN PLACE BY THE CONTRACTOR BEFORE ALLOWING TRAFFIC ON THAT PORTION OF ROADWAY. IF NO PAY ITEM IS PROVIDED FOR THESE ITEMS IN THE BIDDING SCHEDULE, THEN COST OF SUCH ITEMS SHALL BE INCLUDED IN MOBILIZATION.

- SITE A
- SITE B
- SITE C
- GRADE
- PAVEMENT
- SURFACE
- STRUCTURE
- NEOSURFACE
- SUBWAY, SIGN, LIGHT, DELINEATOR
- PAVEMENT MARKING
- SIGNAL
- MAINTENANCE OFF HIGHWAY
- MAINTENANCE ON HIGHWAY

LOCATION OR WORK TYPE	SITE A	SITE B	SITE C	GRADE	PAVEMENT	SURFACE	STRUCTURE	NEOSURFACE	SUBWAY, SIGN, LIGHT, DELINEATOR	PAVEMENT MARKING	SIGNAL	MAINTENANCE OFF HIGHWAY	MAINTENANCE ON HIGHWAY
LANE CLOSURE DIVIDED HIGHWAY (DETAIL A SHEET 4A)													
LANE CLOSURE MULTILANE UNDIVIDED (DETAIL B SHEET 1A)													
LANE CLOSURE - 2 LANE - 2 WAY (DETAIL D - SHEET 4B)			X		X								
LANE CLOSURE MULTIPLE LANES CLOSED (DETAIL K SHEET 4F)													
ROADWAY CLOSED DIVIDED HIGHWAY (DETAIL C SHEET 4D)													
MEDIAN CROSSOVER (DETAIL F SHEET 4C)													
SHOULDER CLOSURE (DETAIL M - SHEET 4G)			X		X								
DETOUR URBAN WITH CIRCULATION													
DETOUR 2 LANE (BRIDGE ETC.) (DETAIL H - SHEET 4D)													
HAUL ROAD CROSSING (DETAIL E SHEET 4E)													
PEDESTRIAN ROUTING (DETAIL R & G - SHEET 4J)													
FLAGGING (DETAILS C & D - SHEET 4B)			X		X								
HAUL ROAD (DETAIL N - SHEET 4G)													
URBAN INTERSECTION (SHEET 4E)													
PAVEMENT MARKING (DETAIL Q - SHEET 4H)				X				X					
DETOUR 1 LANE 2 WAY - SHEET 4K	X		X	X									

TRAFFIC CONTROL

- ~~LANE CLOSURE DIVIDED HIGHWAY (DETAIL A SHEET 4A)~~
- ~~LANE CLOSURE MULTILANE UNDIVIDED (DETAIL B SHEET 1A)~~
- LANE CLOSURE - 2 LANE - 2 WAY (DETAIL D - SHEET 4B)
- ~~LANE CLOSURE MULTIPLE LANES CLOSED (DETAIL K SHEET 4F)~~
- ~~ROADWAY CLOSED DIVIDED HIGHWAY (DETAIL C SHEET 4D)~~
- ~~MEDIAN CROSSOVER (DETAIL F SHEET 4C)~~
- SHOULDER CLOSURE (DETAIL M - SHEET 4G)
- ~~DETOUR URBAN WITH CIRCULATION~~
- ~~DETOUR 2 LANE (BRIDGE ETC.) (DETAIL H - SHEET 4D)~~
- ~~HAUL ROAD CROSSING (DETAIL E SHEET 4E)~~
- ~~PEDESTRIAN ROUTING (DETAIL R & G - SHEET 4J)~~
- FLAGGING (DETAILS C & D - SHEET 4B)
- ~~HAUL ROAD (DETAIL N - SHEET 4G)~~
- ~~URBAN INTERSECTION (SHEET 4E)~~
- PAVEMENT MARKING (DETAIL Q - SHEET 4H)
- DETOUR 1 LANE 2 WAY - SHEET 4K

UTAH DEPARTMENT OF TRANSPORTATION

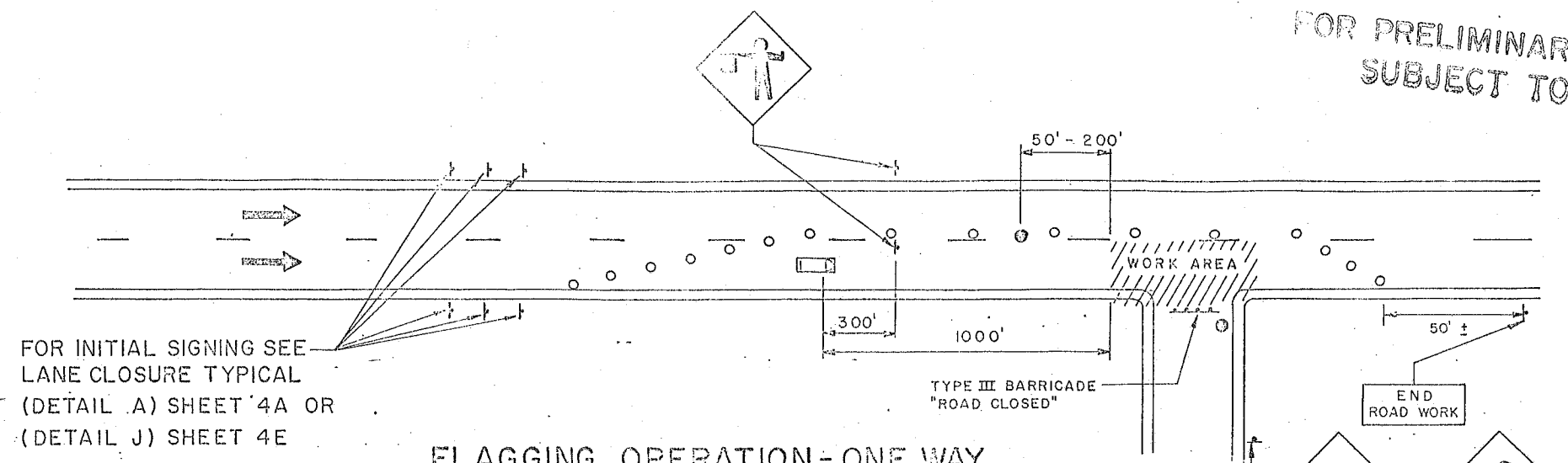
TRAFFIC CONTROL MATRIX AND PHASING DETAIL

FHWA APPROVED		DATE		APPROVED
REVISIONS				
DATE	APPR	DATE	BY	
5/11/88	X	5/12/88	X	
1/12/89	X			

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SUBJECT TO REVISION

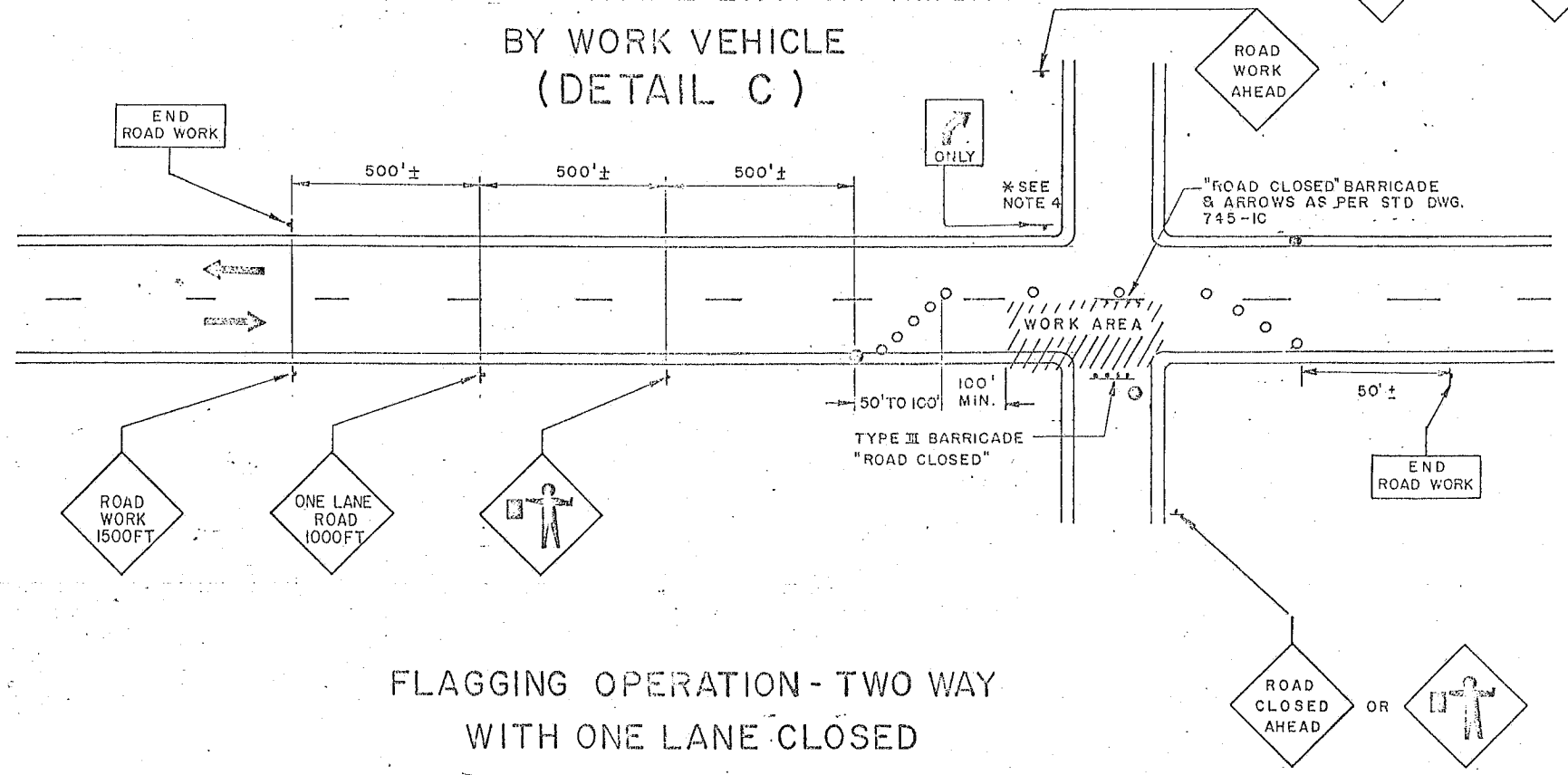
NOTES:

1. DEVICE SPACING AS PER STD. DWG. 745-1A
2. TAPER FORMULA: (UNLESS OTHERWISE NOTED)
 $L = S \times W$ FOR SPEEDS OF 45 MPH OR GREATER
 $L = \frac{WS^2}{60}$ FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
 L = MINIMUM LENGTH OF TAPER IN FEET
 S = POSTED SPEED LIMIT IN MILES PER HOUR
 W = WIDTH OF OFFSET IN FEET
3. SIGN SPACING IS SHOWN FOR 55 MPH & MAY BE REDUCED FOR LESSER SPEEDS IF SIGN LEGEND INDICATING DISTANCE IS CHANGED ACCORDINGLY. IN NO INSTANCE SHALL SIGN SPACING BE LESS THAN 300'.
4. FLAGGING & SYMBOL SIGN MUST BE PROVIDED IF RIGHT TURNING TRAFFIC CONFLICTS WITH ONE WAY TRAFFIC.



FOR INITIAL SIGNING SEE
LANE CLOSURE TYPICAL
(DETAIL A) SHEET 4A OR
(DETAIL J) SHEET 4E

FLAGGING OPERATION - ONE WAY
WITH POTENTIAL ENCROACHMENT
BY WORK VEHICLE
(DETAIL C)



FLAGGING OPERATION - TWO WAY
WITH ONE LANE CLOSED
(DETAIL D)

SAME SIGN SEQUENCE & SPACING
REQUIRED FOR OPPOSITE DIRECTION
OF TRAFFIC.

LEGEND:

- ADVANCE WARNING DEVICE
- FLAGGING STATION
- CHANNELIZING DEVICE AS PER STD. DWG. 745-1A
- TEMPORARY AND PORTABLE SIGN
- DIRECTION OF TRAFFIC
- TYPE III BARRICADE AS PER STD DWG 745-1C

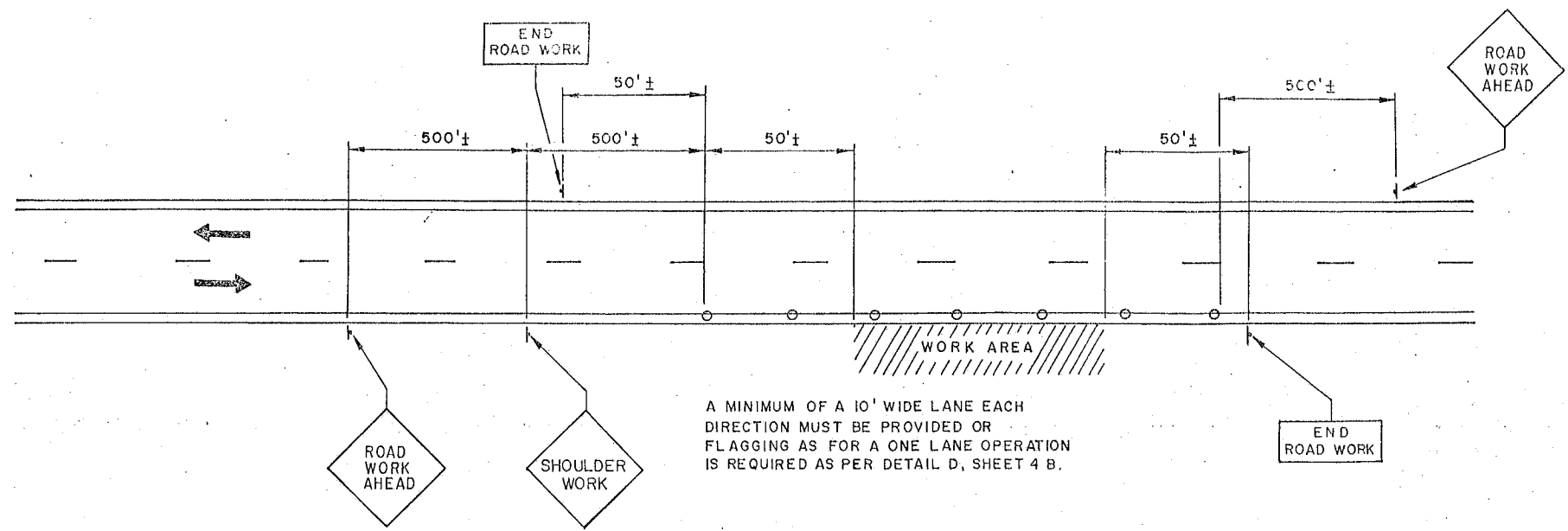
UTAH DEPARTMENT
OF TRANSPORTATION

FHWA APPROVED				RECOMMENDED FOR APPROVAL	
DATE		DATE		DATE	
1/13/82	* APPROVED				
REVISIONS					
UDOT			FHWA		
DATE	APPR.	DATE	APPR.	DESIGN	DATE
8/15/82	*	3/18/82	*	<i>John K. Jaramila</i>	12/21/81
2/14/83	*			<i>John K. Jaramila</i>	12/21/81
3/16/83	*	3/18/83	*	<i>John K. Jaramila</i>	12/21/81
				<i>John K. Jaramila</i>	12/21/81
				<i>John K. Jaramila</i>	12/21/81
				<i>John K. Jaramila</i>	12/21/81

ENGINEER FOR SAFETY
ENGINEER FOR PRECONSTRUCTION APPROVAL
ASSISTANT DIRECTOR

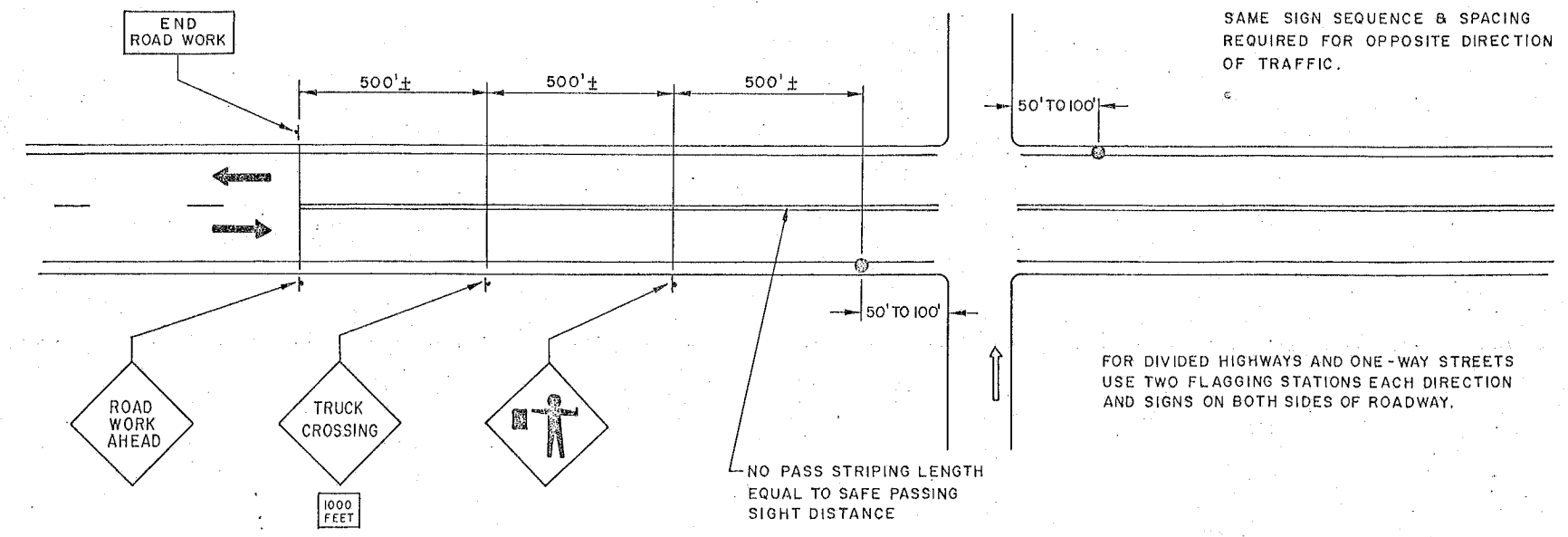
SHEET NO. 4 B

PRELIMINARY PLAN ONLY
 SUBJECT TO REVISION



SHOULDER OPERATION - TWO WAY
 (DETAIL M)

- NOTES:
1. DEVICE SPACING AS PER STD. DWG. 745-1A
 2. TAPER FORMULA: (UNLESS OTHERWISE NOTED)
 $L = S \times W$ FOR SPEEDS OF 45 MPH OR GREATER
 $L = \frac{WS^2}{60}$ FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
 L = MINIMUM LENGTH OF TAPER IN FEET
 S = POSTED SPEED LIMIT IN MILES PER HOUR
 W = WIDTH OF OFFSET IN FEET
 3. SIGN SPACING IS SHOWN FOR 55 MPH & MAY BE REDUCED FOR LESSER SPEEDS IF SIGN LEGEND INDICATING DISTANCE IS CHANGED ACCORDINGLY. IN NO INSTANCE SHALL SIGN SPACING BE LESS THAN 300'.



HAUL ROAD OPERATION - TWO WAY
 (DETAIL N)

- LEGEND:
- ▭ ADVANCE WARNING DEVICE
 - FLAGGING STATION
 - CHANNELIZING DEVICE AS PER STD. DWG. 745-1A
 - ⊖ TEMPORARY AND PORTABLE SIGN
 - ➔ DIRECTION OF TRAFFIC
 - ➡ TRUCK HAUL ROUTE

FHWA APPROVED				REVISIONS				RECOMMENDED FOR APPROVAL	
DATE	APPR.	DATE	APPR.	DATE	APPR.	DATE	APPR.	DESIGN	DESIGN
1/13/82	*	8/18/82	*					<i>Paul K. ...</i>	<i>John ...</i>
						12/28	19 81		
						12/2	19 81		
						12/2	19 81		

UTAH DEPARTMENT OF TRANSPORTATION
 SHOULDER-HAUL ROAD TRAFFIC CONTROL DEVICE LAYOUT

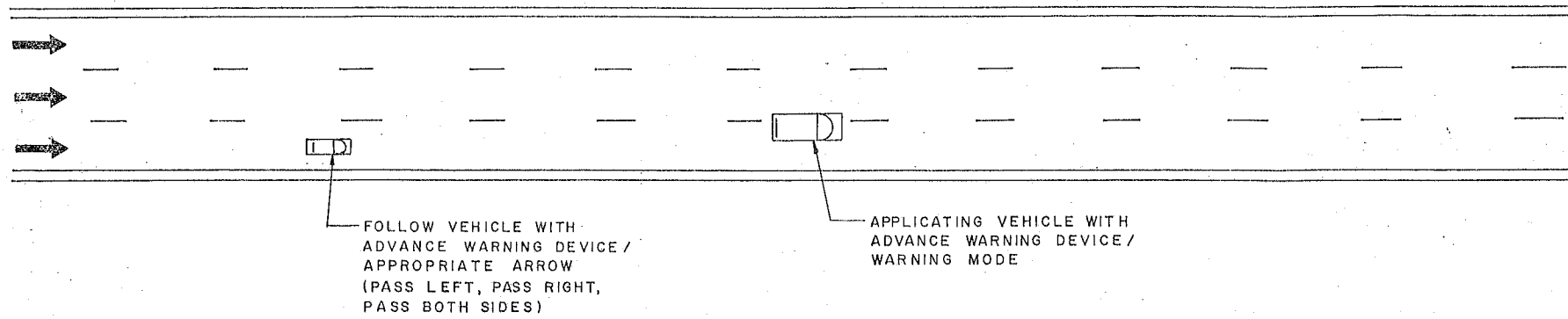
ENGINEER FOR SAFETY: *...* 12/28 19 81
 ENGINEER FOR RECONSTRUCTION: *...* 12/2 19 81
 ASSISTANT DIRECTOR: *...* 12/2 19 81

SHEET NO. 46

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NOTES:

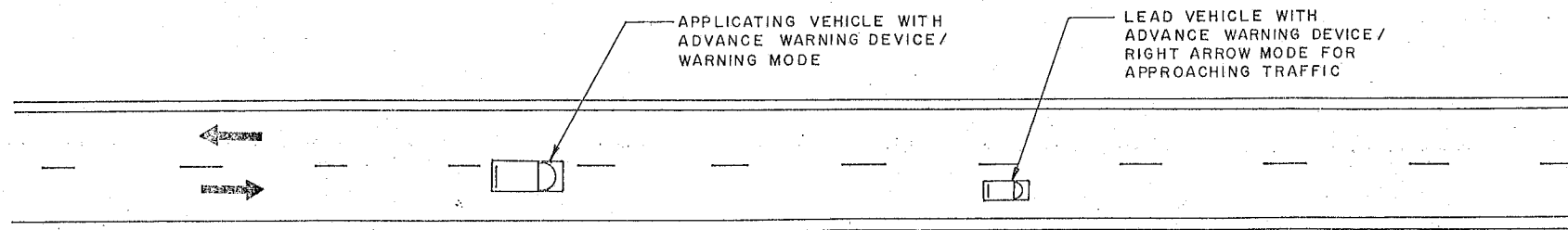
1. DEVICE SPACING AS PER STD. DWG. 745-1A
2. THE DISTANCE BETWEEN THE APPLYING TRUCK AND THE FOLLOW OR LEAD VEHICLE SHOULD NOT BE GREATER THAN 300'
3. NO STATIC SIGNS ARE REQUIRED WITH THESE OPERATIONS.
4. ALL EQUIPMENT SHOULD HAVE ROTARY LIGHTS AND EMERGENCY FLASHERS IN OPERATION



MULTILANE
LANE LINES & SHOULDER
(DETAIL P)

LEGEND:

- APPLYING VEHICLE
- LEAD /FOLLOW VEHICLE WITH ADVANCE WARNING DEVICE
- DIRECTION OF TRAFFIC



2 LANE-TWO WAY
CENTER & SHOULDER
(DETAIL Q)

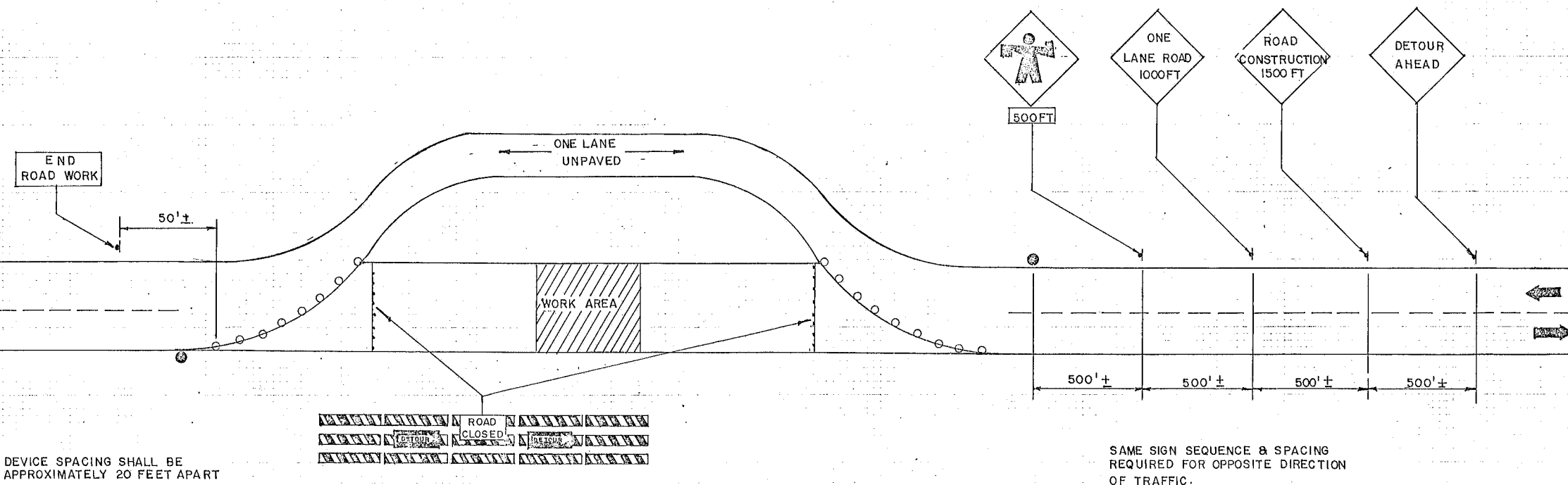
UTAH DEPARTMENT
OF TRANSPORTATION

FHWA APPROVED 8/18/82 DATE				* APPROVED				PAVEMENT MARKING TRAFFIC CONTROL			
REVISIONS								RECOMMENDED FOR APPROVAL			
UDOT				FHWA				DESIGN			
DATE	APPR.	DATE	APPR.	DATE	APPR.	DATE	APPR.	DATE	APPR.	DATE	APPR.
								ENGINEER FOR SAFETY Date 19 82			
								ENGINEER FOR PRECONSTRUCTION APPROVED Date 19 82			
								ASSISTANT DIRECTOR Date 19 82			
SHEET NO. 4H											

FOR PRELIMINARY REVIEW ONLY
SUBJECT TO REVISION

NOTE:

1. THE ROAD MAY BE CLOSED FOR A PERIOD OF UP TO 4 HOURS BETWEEN 9:00 AM AND 3:00 PM FOR CONSTRUCTION OF THE BITUMINOUS APPROACH SLAB (SEE DETAIL, SHEET 2). IF THE ROAD IS CLOSED, THE DETOUR MAY BE UNNECESSARY. NOTIFY SETH BIRD, CACHE COUNTY ROAD SUPERINTENDENT, AT LEAST 24 HOURS PRIOR TO ANY ROAD CLOSURE.
2. DETOUR TO BE USED IN DAYTIME ONLY. TWO LANE TWO WAY TRAFFIC MUST BE PROVIDED AT NIGHT, WITH DETOUR SIGNS REMOVED.
3. AT LEAST ONE LANE MUST BE OPEN AT ALL TIMES EXCEPT FOR CLOSURE PERMITTED IN NOTE 1.
4. ROAD CLOSED BARRICADE STRIPES AND DETOUR ARROW TO BE PROPERLY ORIENTED FOR APPROACHING TRAFFIC.
5. DETOUR CONSTRUCTION AND REMOVAL SHALL BE PAID FOR AS PART OF MOBILIZATION.



DEVICE SPACING SHALL BE APPROXIMATELY 20 FEET APART

SAME SIGN SEQUENCE & SPACING REQUIRED FOR OPPOSITE DIRECTION OF TRAFFIC.

LEGEND

- ⊙ FLAGGING STATION
- CHANNELIZING DEVICE AS STD. DWG. 745-1A
- ▼ TEMPORARY AND PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC

1 LANE 2 WAY DETOUR

UTAH DEPARTMENT OF TRANSPORTATION

RAILROAD CROSSING IMPROVEMENT
2500 NORTH 400 WEST, LOGAN

TRAFFIC CONTROL

PROJECT NUMBER RRS-0005(6)

CACHE COUNTY

SHEET No. 4K

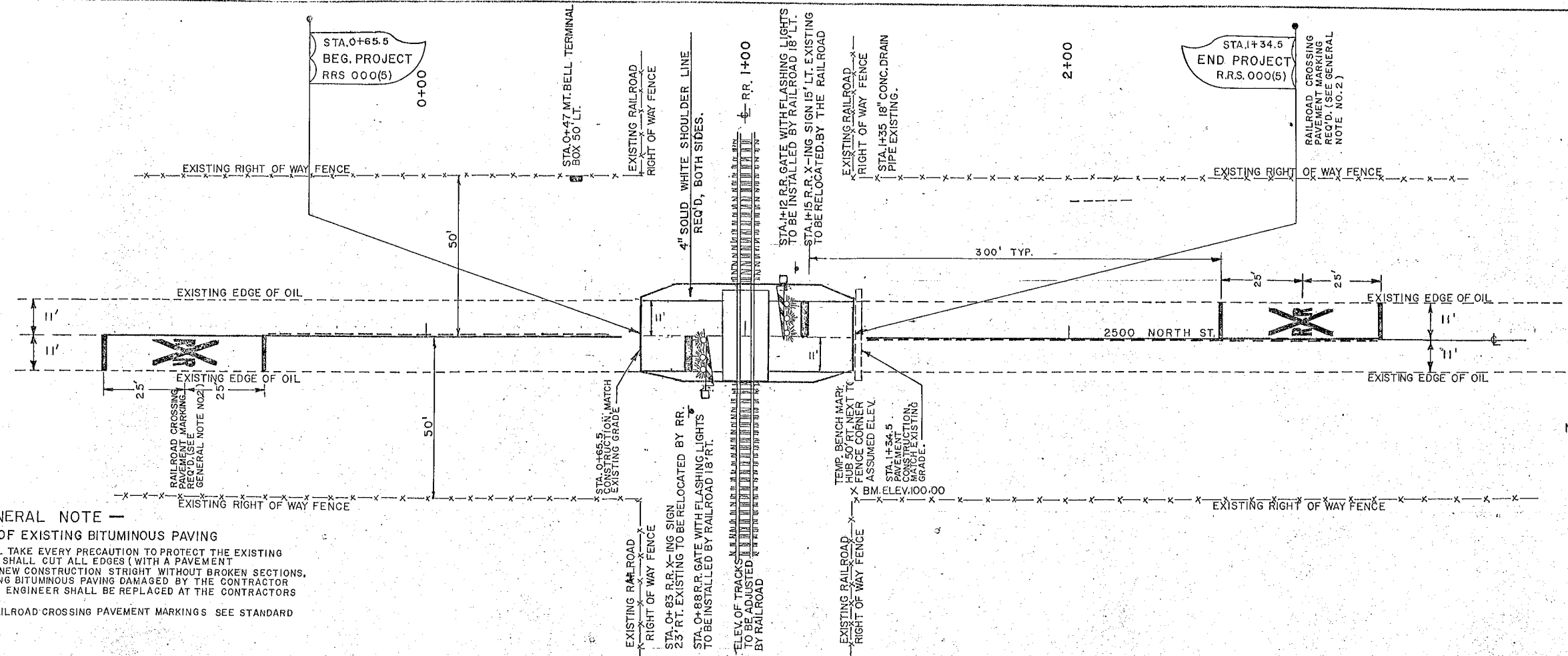
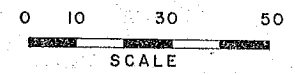
DESIGN W.A.B. 10/84	CHECK	REVIEW
DRAWN J.M.M. 10/84	CHECK	DATE
QUANT. W.A.B. 10/84	CHECK	BY
APPROVAL RECOMM. 11/1/84	PROJECT DESIGN ENGINEER	DATE
APPROVED 11/1/84	DATE	DATE

DESIGN MAP	PARCELS REQUEST	REMARKS
REV. BY	DATE	ORIGINAL SUBMISSION FOR AUTHORIZATION
NO.	DATE	REVISIONS

FOR PRELIMINARY REVISION ONLY

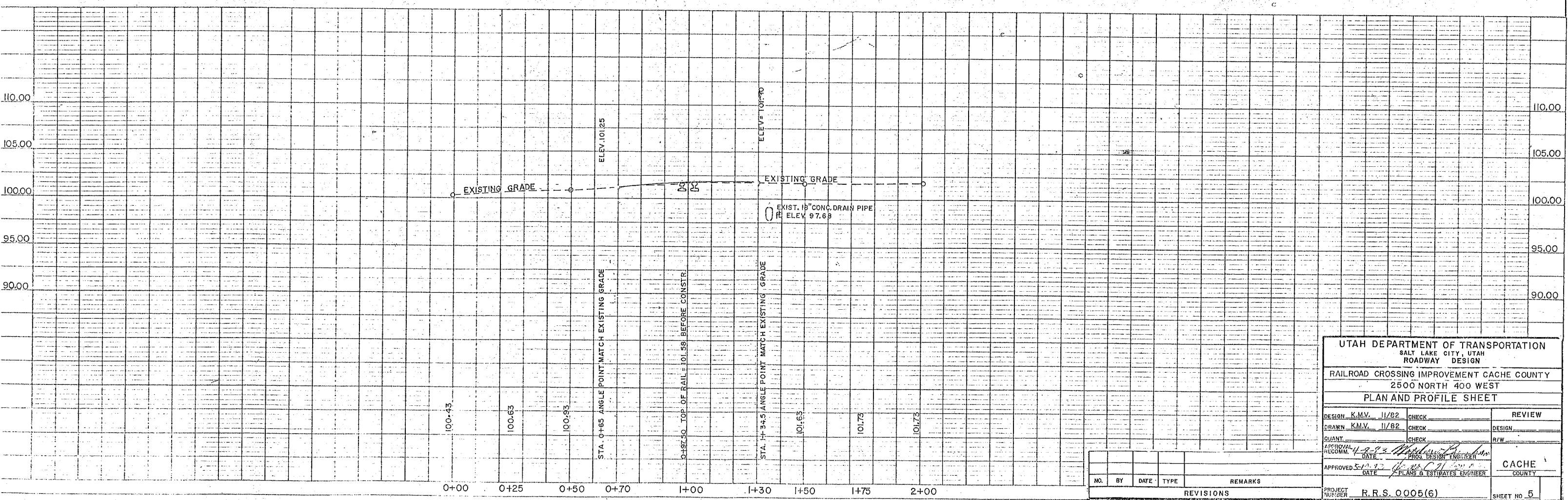


NOTE: CONTRACTOR MUST CONTACT UTILITIES COMPANIES BEFORE DIGGING



GENERAL NOTE

- FOR PROTECTION OF EXISTING BITUMINOUS PAVING**
1. THE CONTRACTOR SHALL TAKE EVERY PRECAUTION TO PROTECT THE EXISTING BITUMINOUS PAVING AND SHALL CUT ALL EDGES (WITH A PAVEMENT SAW) ADJACENT TO THE NEW CONSTRUCTION STRIGHT WITHOUT BROKEN SECTIONS. ALL AREAS OF EXISTING BITUMINOUS PAVING DAMAGED BY THE CONTRACTOR AS DETERMINED BY THE ENGINEER SHALL BE REPLACED AT THE CONTRACTORS EXPENSE.
 2. FOR PLACEMENT OF RAILROAD CROSSING PAVEMENT MARKINGS SEE STANDARD DRAWING NO. 745-47



UTAH DEPARTMENT OF TRANSPORTATION SALT LAKE CITY, UTAH ROADWAY DESIGN			
RAILROAD CROSSING IMPROVEMENT CACHE COUNTY 2500 NORTH 400 WEST			
PLAN AND PROFILE SHEET			
DESIGN	K.M.V.	11/82	CHECK
DRAWN	K.M.V.	11/82	CHECK
QUANT.			CHECK
APPROVAL			CHECK
RECOMM.			CHECK
APPROVED			
DATE			
			REVIEW
			DESIGN
			R/W
			CACHE COUNTY
PROJECT NUMBER			R.R.S. 0005(6)
SHEET NUMBER			5

NO.	BY	DATE	TYPE	REMARKS
REVISIONS				